

No: 25-004

DAUPHIN COUNTY SUBDIVISION/LAND DEVELOPMENT REVIEW REPORT

MunicipalityLower Swatara Township

SurveyorDynamics Survey, LLC

EngineerDynamic Engineering Consultants, PC

Plat TitleProposed McDonald’s Restaurant

Zoning DistrictCommercial Highway (C-H)

Proposed Land Use

McDonald’s restaurant

Plat Status:

Preliminary

Final

X P/F

Minor

Plat Type:

Subdivision

Land Development

Regulations:

County

Municipal

Zoning

S&LD

of Lots2

of New Lots2

of DUs0

Acs Subdiv /Devel0.72

Total Acres0.72

11/26/2024

Staff Review12/9/2024

Official County Review

12/9/2024

Date Received

Reviewed by ARK Checked by Tax Map Parcel # 36-023-017; 36-023-035

Project Description: The project proposes to amend a previously approved land development application to replace the previously proposed QSR restaurant with a McDonald’s restaurant including associated site improvements.

- When applicable, streets, sewer, water, storm drainage, and other infrastructure elements to be verified as adequate by municipal staff/engineer. Any improvement guarantees shall be posted prior to final plat approval.
- Registered PA Land Surveyor is required to certify all lot/tract boundary descriptions.
- When applicable, zoning compliance to be verified by Municipal Zoning Officer.
- Appropriate sewage module component should be processed prior to final plat approval.
- Final plats must be recorded within 90 days of approval.

Review comments with cited ordinance provisions are based on municipal regulations on file with the County Planning Commission.

Waivers Requested: Section 22-404 Preliminary Plat Procedure; Section 22-605.A.2 High-density polyethylene pipe requirements

Comments:

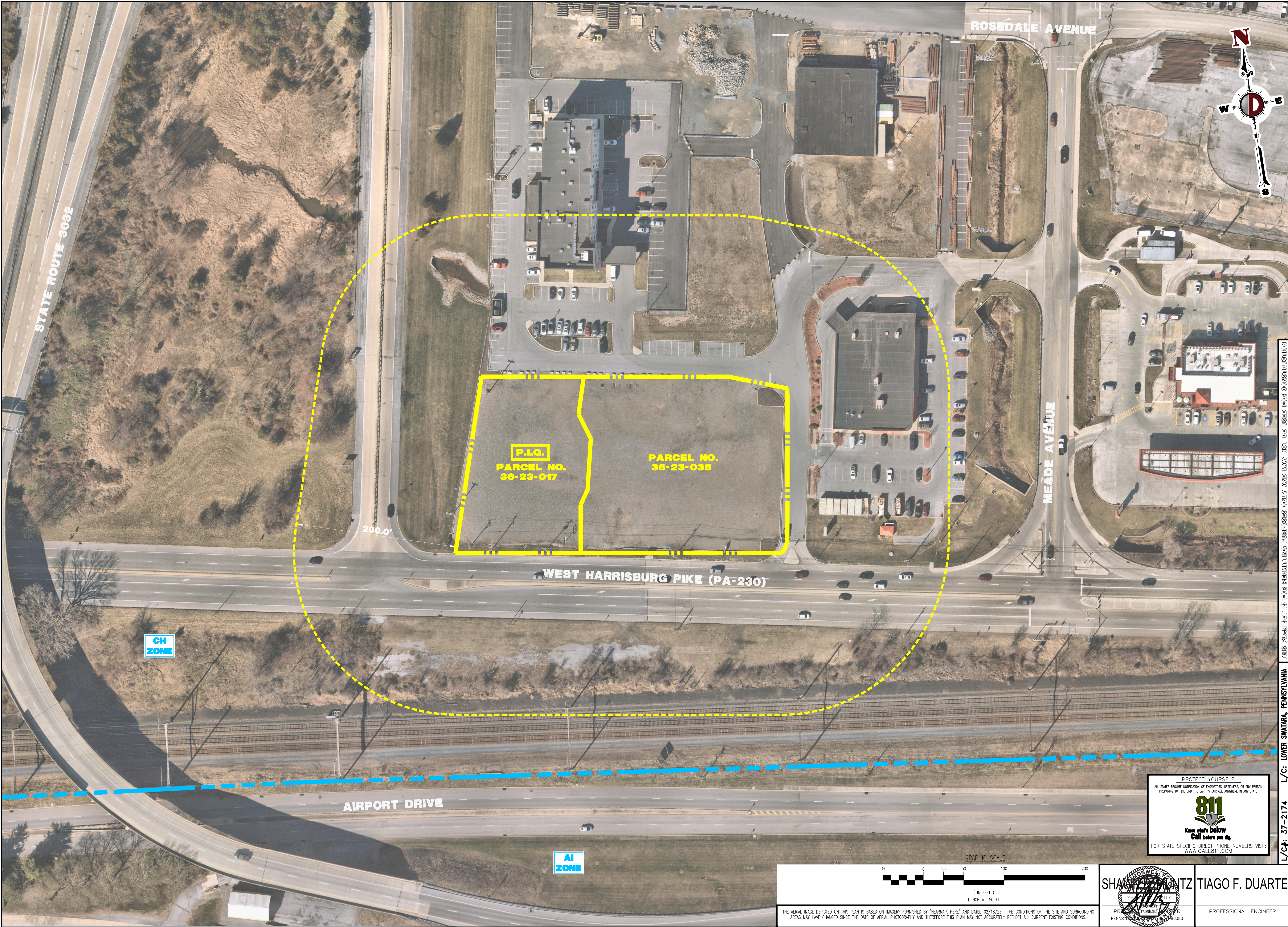
- All signatures, notarizations, and certifications must be obtained before the plan is brought in for signing.
- Verify that a valid Highway Occupancy Permit (HOP) is on file for West Harrisburg Pike (State Route 230).
- Verify that a traffic study has or will be completed.
- Label all permanent monuments and markers.
- Can the applicant explain the circulation of customer vehicles in and out of the site? It appears that regular vehicles will enter and exit by the same route the tractor trailers take, entering off Route 230 via Jamesway Plaza and then exiting the same way?

- Is there any potential conflict between vehicles leaving the site from the parking area versus those leaving the drive-through?
- The parking calculations include both the proposed restaurant and the existing WaWa. Exactly how many new spaces are being added? Which development are they meant to serve?
- County data shows significant floodplain presence on the site. Does the location have any known flooding or existing stormwater/runoff issues that may need special consideration?

Plan No. 25-004

Plat Specifications	Yes	No	N/A
1. Name of proposed subdivision/land development shown	X		
2. Owner/developer name, address & telephone number shown		X	
3. Municipality name shown	X		
4. Tax parcel number/Deed reference shown/Instrument #	X		
5. North point shown	X		
6. Map scale shown (written/graphic)	X		
7. Date of plan preparation shown	X		
8. Certification of surveyor/engineer/landscape architect shown		X	
9. Location map shown	X		
10. Total property map (bearings, distances, area, primary control point) shown	X		
11. Names of adjacent landowners/subdivision shown	X		
12. Lot numbers shown		X	
13. Lot dimensions shown	X		
14. Lot areas shown	X		
15. Permanent monuments and markers shown		X	
16. Building setbacks shown	X		
17. Existing natural features shown - Wetlands		X	
Floodplains		X	
Woodlands, streams, etc.		X	
18. Contours at required interval shown		X	
19. Easements shown and identified	X		
20. Existing man-made features shown - Building (s)	X		
Storm drainage facilities	X		
Sewer mains/Septic	X		
Water mains/Well	X		
21. Proposed man-made features shown – Building (s) / Structure (s)	X		
Storm drainage facilities	X		
Sewer disposal – public (X) on-lot ()	X		
Water supply – public (X) well ()	X		
22. Existing streets shown - Name	X		
R/W width	X		
Paving width	X		
Dedicated R/W width	X		
23. Proposed streets shown - Name			X
R/W width			X
Paving width			X
Profiles			X
24. Curbs shown	X		
25. Sidewalks shown	X		
26. Existing and proposed coverage shown	X		
27. Parking schedule provided shown	X		
28. Traffic study completed Verify		X	
29. Recreation area shown/fee in-lieu-of provided			X
30. Erosion and sedimentation control plan shown	X		
31. Statement of ownership, signature and notarization shown Need signature and notarizations.		X	
32. Dedicatory statement shown Need signature	X		
33. Approval blocks shown	X		
34. PADOT Highway Occupancy Permit statement shown		X	
35. Consistency with Future Land Use plans - County plans	X		
Municipal plans	X		

Plotter: 11/19/24 - 3:22 PM, Br. modeler, Product Ver. 24.3e (LMS Tech)
File: P:\DECHC PROJECTS\0114 McDonald's 23-01896 Lower Swatara PA LC 37-2174\Aerial.dwg, ----> 02 AERIAL MAP



THE AERIAL IMAGE DEPICTED ON THIS PLAN IS BASED ON IMAGERY FURNISHED BY "NEARMAP, HERE" AND DATED 02/18/23. THE CONDITIONS OF THE SITE AND SURROUNDING AREAS MAY HAVE CHANGED SINCE THE DATE OF AERIAL PHOTOGRAPHY AND THEREFORE THIS PLAN MAY NOT ACCURATELY REFLECT ALL CURRENT EXISTING CONDITIONS.

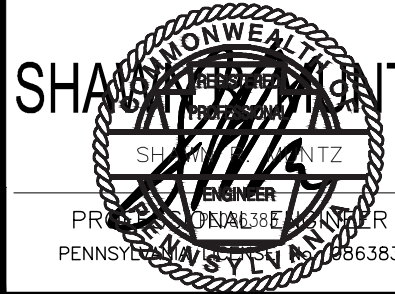
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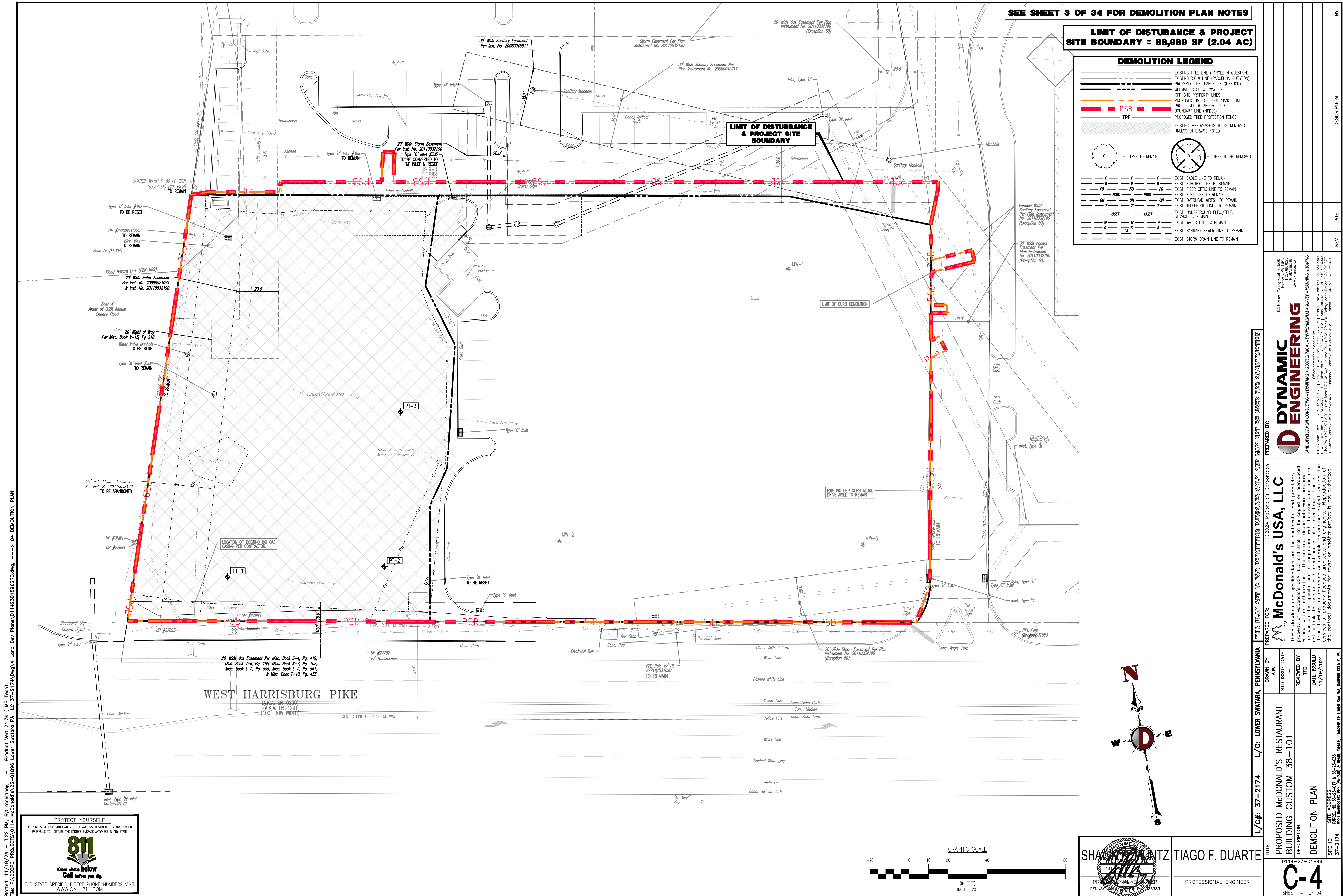
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PROFESSIONAL ENGINEER

L/C#: 37-2174		L/C: LOWER SWATARA, PENNSYLVANIA		THIS PLAN SET IS FOR PERMITTING PURPOSES ONLY AND MAY NOT BE USED FOR CONSTRUCTION	
DRAWN BY AJW		PREPARED FOR: © 2024 McDonald's Corporation		PREPARED BY: 825 Newton Valley Road, Suite 201 Newtown, PA 18940 P: 207.585.0361 www.dynanet.com	
PROPOSED McDONALD'S RESTAURANT BUILDING CUSTOM 38-101		McDonald's USA, LLC		 DYNAMIC ENGINEERING LAND DEVELOPMENT CONSULTING • PERMITTING • GEOTECHNICAL • ENVIRONMENTAL • SURVEY • PLANNING & ZONING	
DESCRIPTION		These drawings and specifications are the confidential and proprietary property of McDonald's USA, LLC and shall not be copied or reproduced in whole or in part without the written consent of McDonald's USA, LLC. Use of these drawings on this specific site in conjunction with its issue date and are not suitable for use on a different site or at a later time.		Office: 825 Newton Valley Road, Suite 201 Newtown, PA 18940 P: 207.585.0361 www.dynanet.com	
AERIAL MAP		DATE ISSUED 11/19/2024		Lobsenz, New Jersey P: 202.974.0111 • Ayrton, New Jersey P: 954.354.2000 Newtown, New Jersey P: 207.585.0361 • Toms River, New Jersey P: 732.974.0119 • Annapolis, Maryland P: 410.541.5000 Newtown, Pennsylvania P: 207.585.0361 • Philadelphia, Pennsylvania P: 215.252.4881 • Baltimore, Maryland P: 410.578.4420	
SITE ID 37-2174		SITE ADDRESS 101 & 20-03 WEST HARRISBURG, PA 17103		REV DATE DESCRIPTION	



Commonwealth Of Pennsylvania
Department Of Transportation
Deed Book P 15, Pg. 713
Parcel 36-023-PDH

PARCEL 36-023-038-000-0000
N/F LINDEN CENTRE LLC
DB. N/A, PG. N/A

SEE SHEET 3 OF 34 FOR SITE PLAN NOTES

Plotted: 11/19/24 - 3:23 PM, By: mdlaney, - Product Ver: 24.3s (LMS Tech)
 File: P:\DECPC PROJECTS\0114 McDonald's\23-01896 Lower Swatara PA LC 37-217A\Draw\4 Land Dev Plans\01142301896SSO.dwg, ----> 05 SITE PLAN

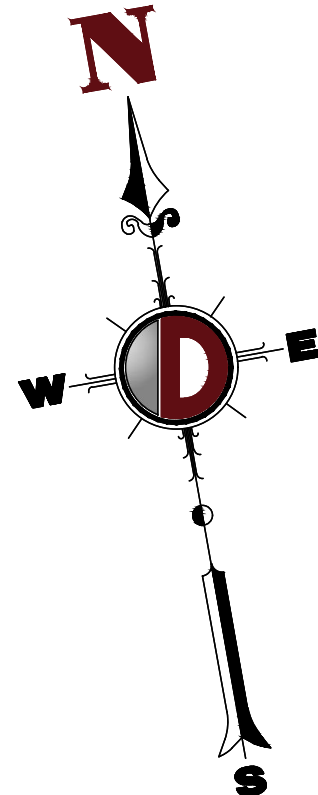
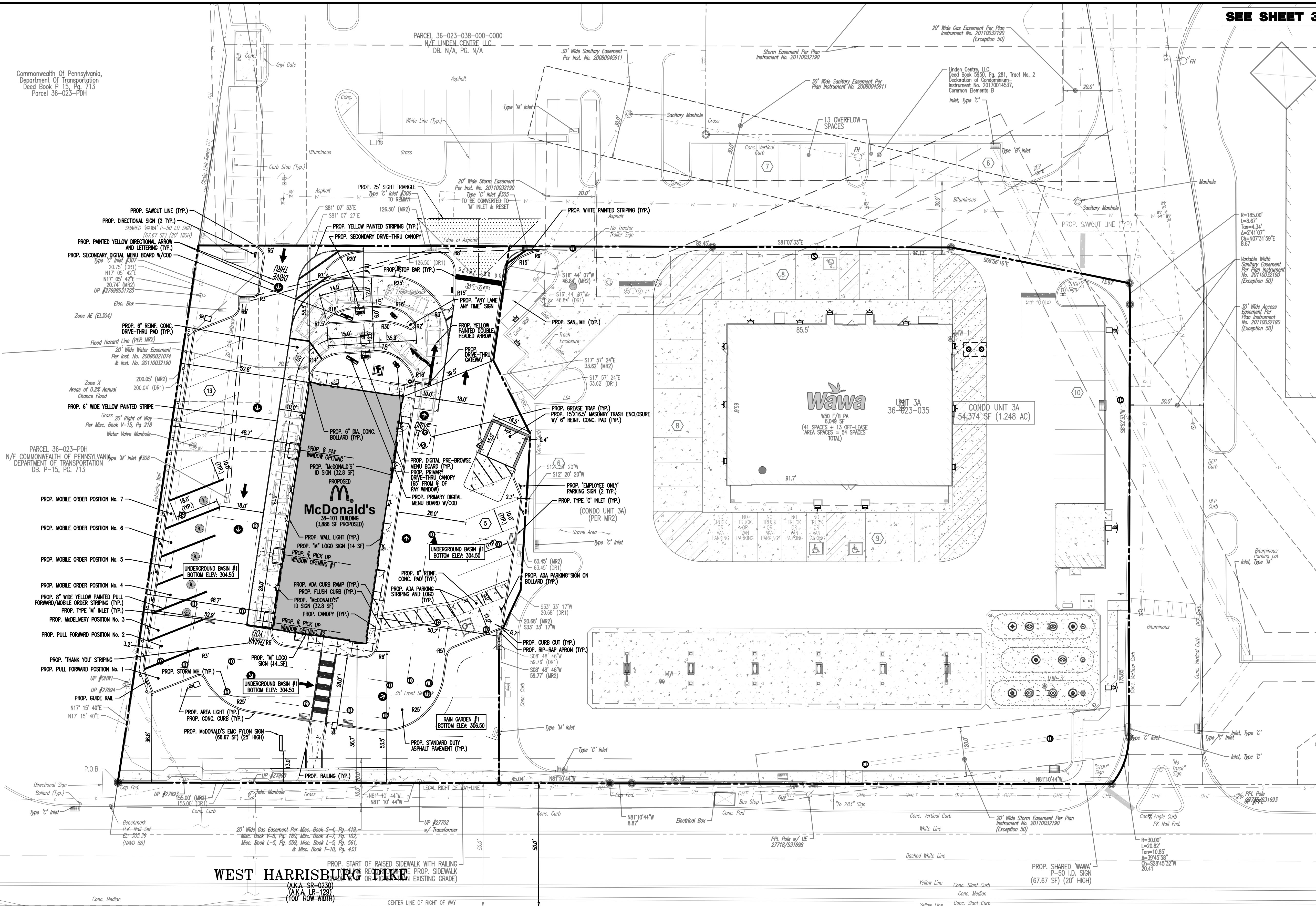
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SITE PLAN APPROVAL		
I/C#	PCA#	
	INITIAL	DATE
DEVELOPMENT		
ACM		
ARM		
RCM		
PRSM		
RCD		
OPERATIONS		
FSM		
DO		
VP USC		
OWNER OPERATOR		
BY:		

SHAHEEN M. M. SHAHEEN
 COMMONWEALTH OF PENNSYLVANIA
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 ENGINEER
 PROFESSIONAL ENGINEER
 PENNSYLVANIA
 13663

PROFESSIONAL ENGINEER

SHANMUNU ENTZ | TIAGO F. DUARTE

TITLE	DRAWN BY

PROPOSED McDONALD'S RESTAURANT
BUILDING CUSTOM 38-101

SITE PLAN

SITE ID	SITE ADDRESS
37-2174	PARCEL NO. 36-23-017 & 36-23-035

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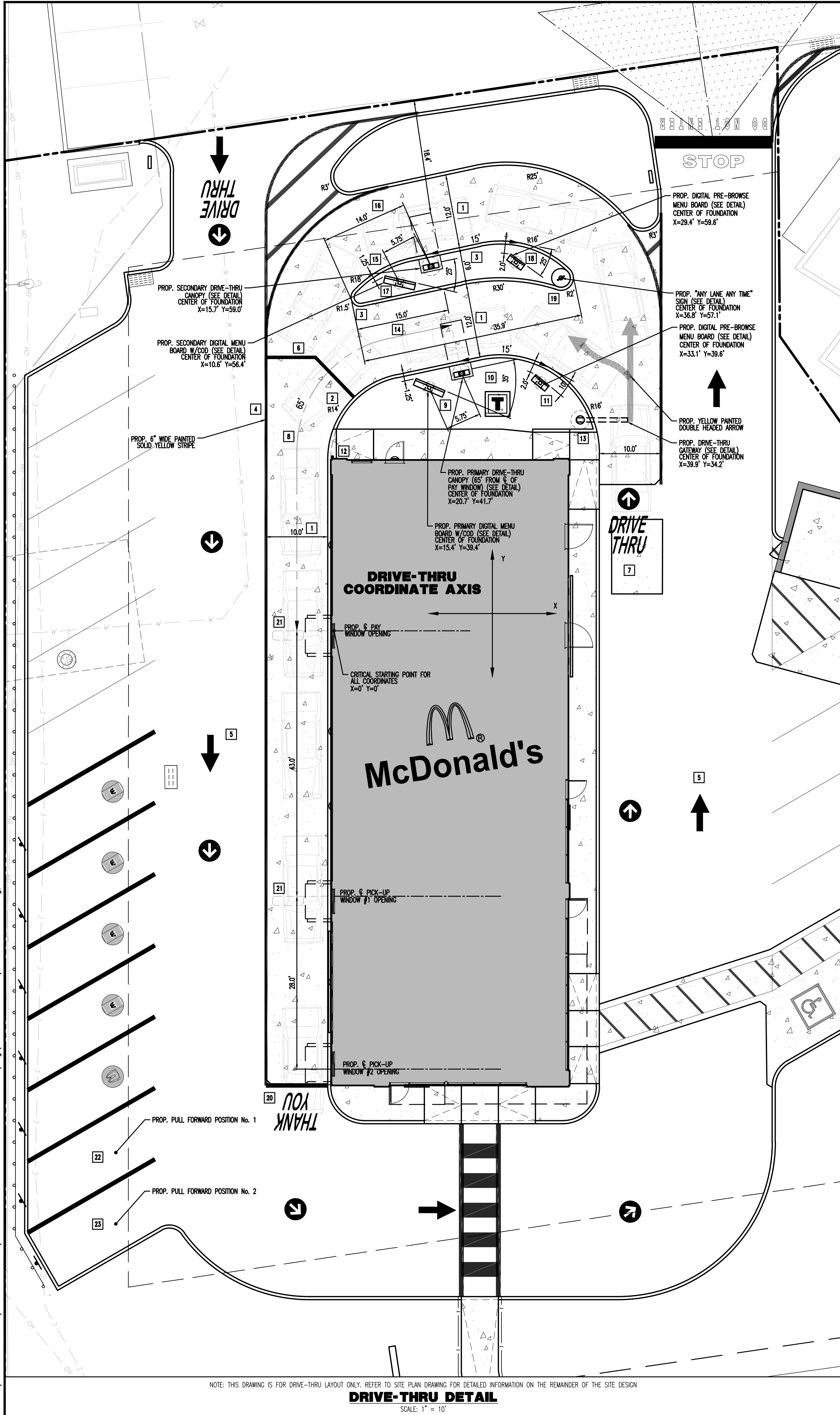
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REV	DATE	DESCRIPTION
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BY _____



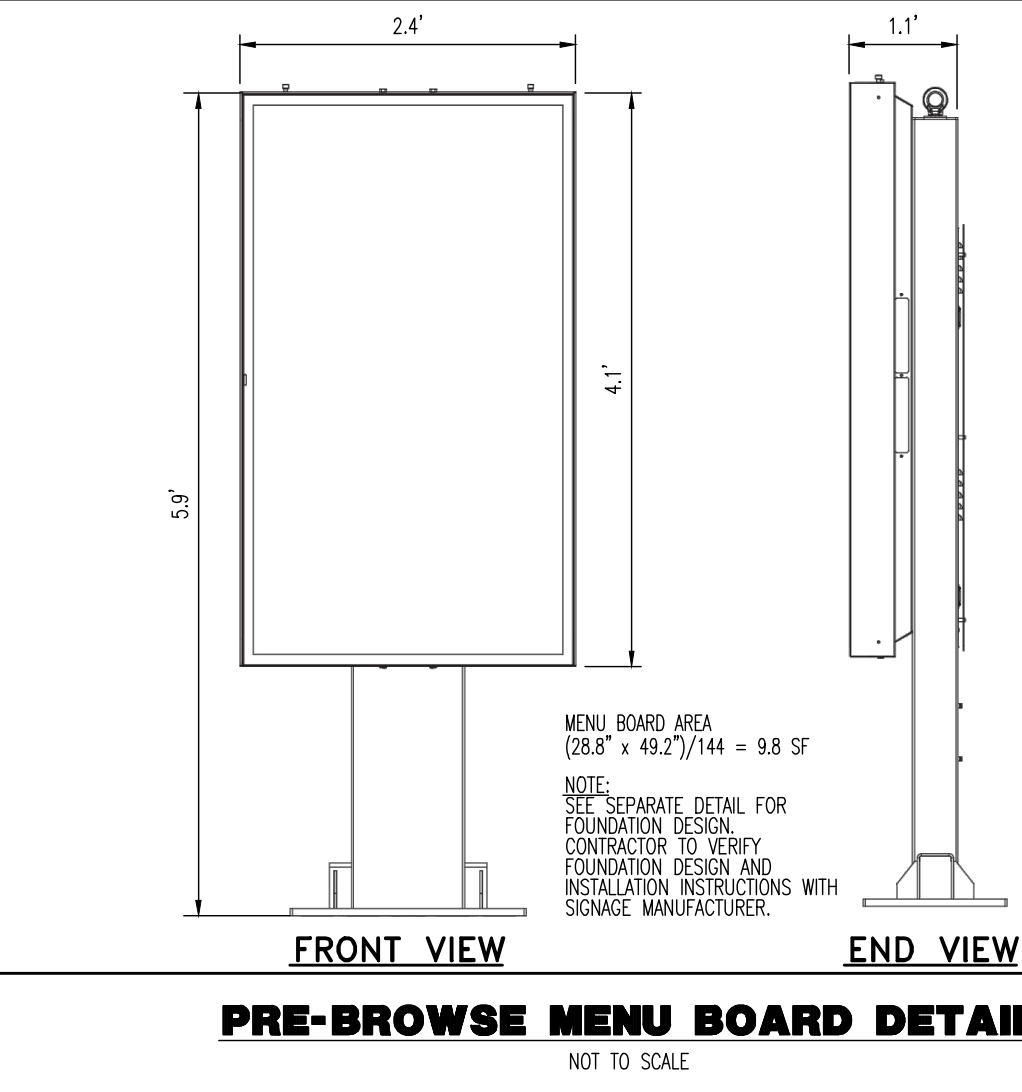
NOTE: THIS DRAWING IS FOR DRIVE-THRU LAYOUT ONLY. REFER TO SITE PLAN DRAWING FOR DETAILED INFORMATION ON THE REMAINDER OF THE SITE DESIGN
DRIVE-THRU DETAIL
SCALE: 1" = 10'

SIDE-BY-SIDE DRIVE-THRU CRITERIA NOTES

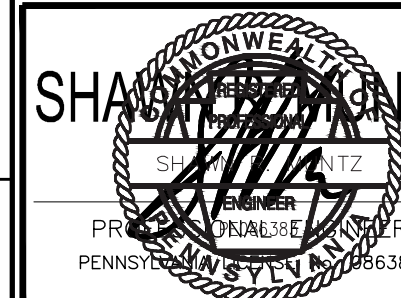
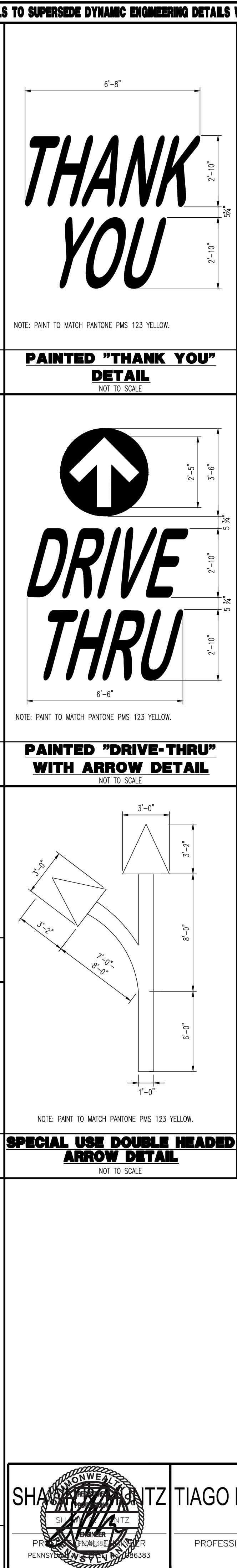
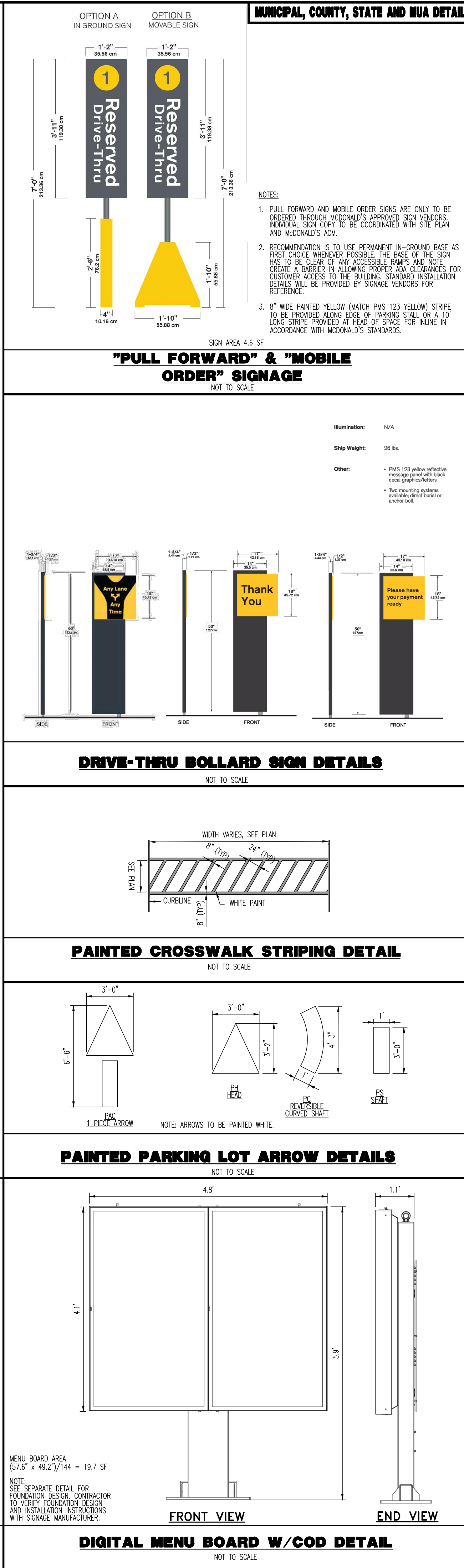
- DRIVE-THRU LANES BOUND BY CURB ON BOTH SIDES ARE TO BE 12'-0". LANES BOUND BY CURB ON ONE SIDE AND PAINTED STRIPING ON THE OTHER SIDE ARE TO BE A MIN. OF 10'-0".
- THE MIN. RADIUS FOR ALL INSIDE/DRIVER'S SIDE DRIVE-THRU CURBING IS 14'-0".
- THE OVERALL LENGTH OF THE CURBED ISLAND SHOULD BE 35'-45". THE LENGTH OF THE ISLAND FROM THE CANOPY ALLOWS FOR THREE CARS IN THE SECONDARY LANE, TWO IN THE PRIMARY LANE AND ONE AT THE COMMITMENT POINT. THE RADIUS FOR THE ISLAND TIP SHALL BE 1'-6". THE ISLAND SHALL BE 6'-0" AT THE WIDEST POINT (FACE OF CURB TO FACE OF CURB).
- 6" WIDE YELLOW PAINT STRIPE TO SPAN OUTER EDGE OF THE ENTIRE DRIVE-THRU LANE.
- ARROW PAVEMENT MARKING. STANDARD STRIPING MARKINGS ARE 7'-0" SHAFT, 7'-0" ARROW STEM AND 3'-0" FOR THE ARROW HEAD. TIP OF ARROW HEAD TO BE LOCATED AT CENTER OF EACH LANE.
- MERGE POINT IS LOCATED WHERE TWO VEHICLES LEAVING EACH CANOPY SIMULTANEOUSLY MEET. THE MERGE POINT STRIPING IS TO BE LOCATED BY OFFSETTING THE INNER PRIMARY LANE BACK OF CURB 9'-0" AND OFFSETTING THE OUTER LANE STRIPING 8'-0". AT THE INTERSECTION OF THESE OFFSETS, A 6" YELLOW STRIPE IS TO BE MARKED PERPENDICULAR TO THE OUTER LANE AS WELL AS THE INNER PRIMARY LANE.
- A CIRCLE DIRECTIONAL ARROW CENTERED ABOVE THE WORD "DRIVE THRU" USED TO INDICATE THE DRIVE THRU ENTRY POINT.
- MIN. 60'-0" (45', 60'-65') LINEAR DISTANCE BETWEEN THE CENTER LINE OF THE CANOPY FACE AND THE CENTER LINE OF THE OPEN ORDER WINDOW AS MEASURED ALONG THE CENTER LINE OF THE LANE. THIS MAY ONLY BE INCREASED IN 20'-0" INCREMENTS (± FOR 80', 100', AND 120') TO A MAX OF 120'. 100' IS OPTIMAL.
- THE CENTER OF THE PRIMARY MENU BOARD FOUNDATION IS TO BE 5'-0" (5'-0" MIN. AND 6'-0" MAX.) FROM THE CENTER OF THE CANOPY FOUNDATION. WITH THE END CAP OF THE PRIMARY MENU BOARD NOT LESS THAN 15" FROM FACE OF CURB. THE PRIMARY MENU BOARD SHOULD BE AT AN ANGLE OF APPROXIMATELY 25° TO 35° ANGLE (35° PREFERRED) FROM A CAR POSITIONED AT THE CANOPY AND WITH 100% VISIBILITY. THE PRIMARY LANE DETECTOR LOOP SHOULD BE PERPENDICULAR TO THE CENTER OF THE PRIMARY CANOPY.
- AUGER "MCDONALD'S" ORDER HERE CANOPY DRIVE-THRU CANOPY FOUNDATION TIGHT AGAINST BACK OF CURB. SEE MANUFACTURER/LOCAL SPECIFICATIONS FOR DETAILS.
- PRE-BROWSE BOARD MUST BE MIN. 12" FROM FACE OF CURB (18" TO 24" PREFERRED). THE DISTANCE BETWEEN THE PRIMARY CANOPY AND PRE-BROWSE BOARD IS TO BE 15' AS MEASURED ALONG THE FACE OF THE CURB. THIS IS MEASURED FROM THE CENTER OF THE PRE-BROWSE BOARD FOUNDATION TO THE CENTER OF THE CANOPY FOUNDATION. THE ANGLE (APPROXIMATELY 10°) OF THE PRE-BROWSE BOARD SHOULD MAXIMIZE VISIBILITY TO THE SECOND CAR FROM CANOPY.
- A SINGLE BOLLARD SHOULD BE POSITIONED AT THE CORNER OF THE BUILDING ON THE DRIVE-THRU SIDE. IT SHOULD BE FLUSH AGAINST THE BUILDING AND FACE OF THE BOLLARD SHOULD BE TIGHT AGAINST THE BACK OF THE CURB.
- AUGER "MCDONALD'S" GATEWAY SIGN FOUNDATION TIGHT AGAINST BACK OF CURB. SEE MANUFACTURER/LOCAL SPECIFICATIONS FOR DETAILS.
- THE DISTANCE BETWEEN THE TIP OF THE CURBED ISLAND AND THE CENTER LINE OF THE PRIMARY CANOPY MUST BE 15'-0". THIS MEASUREMENT IS TAKEN PARALLEL TO THE INSIDE CURB FACE OF THE PRIMARY LANE.
- TO POSITION THE SECONDARY CANOPY, DRAW AN ARC WITH A 14' RADIUS THAT IS CENTERED FROM THE MIDPOINT OF THE ISLAND TIP. THEN OFFSET THE FACE OF THE CURB BY 24" TO DETERMINE THE LOCATION OF CENTER OF FOUNDATION OF THE SECONDARY CANOPY.
- WHEN THE SECONDARY CANOPY IS LOCATED AT 14'-0" FROM THE TIP OF THE CURBED ISLAND, THE LOOP DETECTOR IS TO BE 2'-0" FORWARD OF THE CANOPY CENTER LINE WITH THE LOOP FACING FORWARD AND THE DETECTOR LOOP PERPENDICULAR TO THE SECONDARY CANOPY WHEN POSSIBLE.
- THE CENTER OF THE SECONDARY MENU BOARD FOUNDATION SHALL BE 5'-9" (5'-6" MIN. AND 6'-0" MAX.) FROM CENTER OF THE CANOPY FOUNDATION. WITH THE END CAP OF THE SECONDARY MENU BOARD NOT LESS THAN 15" FROM FACE OF CURB. THE SECONDARY MENU BOARD SHOULD BE AT AN ANGLE OF APPROXIMATELY 25° FROM A VEHICLE POSITIONED AT THE CANOPY AND WITH 100% VISIBILITY.
- PRE-BROWSE BOARD MUST BE MIN. 12" FROM FACE OF CURB (18" TO 24" PREFERRED). THE DISTANCE BETWEEN THE SECONDARY CANOPY AND PRE-BROWSE BOARD IS TO BE 15' AS MEASURED ALONG FACE OF THE CURB. THIS IS MEASURED FROM THE POINT PERPENDICULAR TO THE CENTER OF THE PRE-BROWSE BOARD FOUNDATION TO THE POINT PERPENDICULAR TO THE CENTER OF THE CANOPY FOUNDATION. THE ANGLE OF THE PRE-BROWSE BOARD SHOULD MAXIMIZE VISIBILITY TO THE SECOND CAR FROM CANOPY (PREFERRED 35°).
- "ANY LANE, ANY TIME" BOLLARD SIGN MUST BE A MIN. OF 1'-6" FROM FACE OF CURB AT THE BEGINNING OF THE LANDSCAPE ISLAND. BOLLARD SIGN IS TO BE ORIENTED AT AN ANGLE OF 90° FROM THE CURB.
- THE WORDS "THANK YOU" ARE TO BE PLACED 10" FROM THE CENTER LINE OF THE OPEN PICKUP WINDOW OR 8" FROM THE EDGE OF THE YELLOW STRIPE TO THE BOTTOM OF THE WORD "YOU".
- DETECTOR LOOPS SHALL BE LOCATED AT THE CENTER OF THE OPENING AT THE PAY AND PICKUP WINDOWS.
- PULL FORWARD POSITION NO. 1. PROVIDE 8" WIDE, 10' LONG, PAINTED (PMS 123 YELLOW) STRIPE 40' FROM CENTER OF OPEN PRESENT WINDOW AND PULL FORWARD IN-GROUND SIGN.
- PULL FORWARD POSITION NO. 2. PROVIDE 8" WIDE, 10' LONG, PAINTED (PMS 123 YELLOW) STRIPE 40' FROM PULL FORWARD POSITION NO. 1 STRIPE AND PULL FORWARD IN-GROUND SIGN.

GENERAL DRIVE-THRU NOTES

- SIGNAGE & DRIVE-THRU ELEMENTS. DRIVE-THRU CANOPY, GATEWAY, MENU BOARD AND PRE-BROWSE BOARD SHALL BE CONSISTENT WITH THE CURRENT STANDARD BUILDING DESIGN DRIVE-THRU ELEMENTS. OTHER DESIGNS MAY NOT BE USED.
- THE PLACEMENT OF THE CANOPY AND ANY ADDITIONAL EQUIPMENT SHOULD BE SUCH THAT IT PREVENTS BLOCKING THE CUSTOMER'S VIEW OF THE MENU BOARD WHILE ORDERING.
- GENERAL CONTRACTOR SHALL COORDINATE WITH CIVIL PLANS, MCDONALD'S PROJECT MANAGER AND SIGNAGE SUPPLIER TO CONFIRM EXACT LOCATION, ORIENTATION, MOUNTING HEIGHTS AND NUMBER OF SIGNS AND OTHER DRIVE-THRU ELEMENTS TO BE INSTALLED AT THIS SITE PRIOR TO CONSTRUCTION. ALL WORK TO BE COORDINATED WITH OTHER TRADES.
- CONTACT MCDONALD'S AREA CONSTRUCTION MANAGER FOR SIGNAGE & DRIVE-THRU ELEMENT FOOTING AND WIRING REQUIREMENTS. SIGNAGE MANUFACTURER TO PROVIDE FOOTING ANCHORS & TEMPLATES TO G.C. PRIOR TO FOUNDATION POURING.
- SEE ARCHITECTURAL PLANS FOR DRIVE-THRU LOOP DETECTOR AND WIRING INFORMATION.
- GENERAL CONTRACTOR TO COORDINATE THE RESPONSIBILITIES OF THE ELECTRICAL CONTRACTOR AND SIGN SUPPLIER.
- GENERAL CONTRACTOR TO INSTALL PRE-FORMED, PRE-WIRED VEHICLE DETECTOR LOOP. SEE ARCHITECTURAL PLANS FOR DETAILS.
- GENERAL CONTRACTOR TO VERIFY CONDUIT SIZES REQUIRED BY VEHICLE LOOP DETECTOR SUPPLIER.
- IF 15' MIN. CLEARANCE CANNOT BE ATTAINED OR IF THERE IS A HIGH CHANCE OF AN IMPACT, A BOLLARD MAY BE INSTALLED TO PROTECT THE APPROPRIATE BOARD(S). 100% VISIBILITY OF THE BOARD(S) IS REQUIRED AFTER BOLLARD PLACEMENT.
- ALL DRIVE-THRU EQUIPMENT SUPPLIED BY MCDONALD'S APPROVED SUPPLIERS.



PRE-BROWSE MENU BOARD DETAIL
NOT TO SCALE



PROFESSIONAL ENGINEER

TIAGO F. DUARTE

C-17		SHEET 17 OF 34		0114-23-01896	
TITLE		DRAWN BY		PREPARED FOR	
PROPOSED McDONALD'S RESTAURANT BUILDING CUSTOM 38-101		STD ISSUE DATE - REVIEWED BY ITD		© 2024 McDonald's Corporation	
CONSTRUCTION DETAILS		DATE ISSUED 11/19/2024		McDonald's USA, LLC	
SITE ID		SITE ADDRESS		These drawings and specifications are the confidential and proprietary property of McDonald's USA, LLC and shall not be copied or reproduced for use on this specific site in conjunction with its issue date and are not suitable for use on a different site or at a later time. Use of these drawings and specifications without the express written consent of McDonald's USA, LLC constitutes a breach of contract. Reproduction of the contract documents for reuse on another project is not authorized.	
37-2174		WEST HARRISBURG TWP & 36-03-02 WEST HARRISBURG TWP & 36-03-02 WEST HARRISBURG TWP & 36-03-02		Office: 200	